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# Central Tri-State Technical Outreach Committee Meeting

Presented by Rocco Zuccherro  
March 2019

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# Feedback On Local Issues

## WHAT ARE YOU HEARING?

Construction impacts

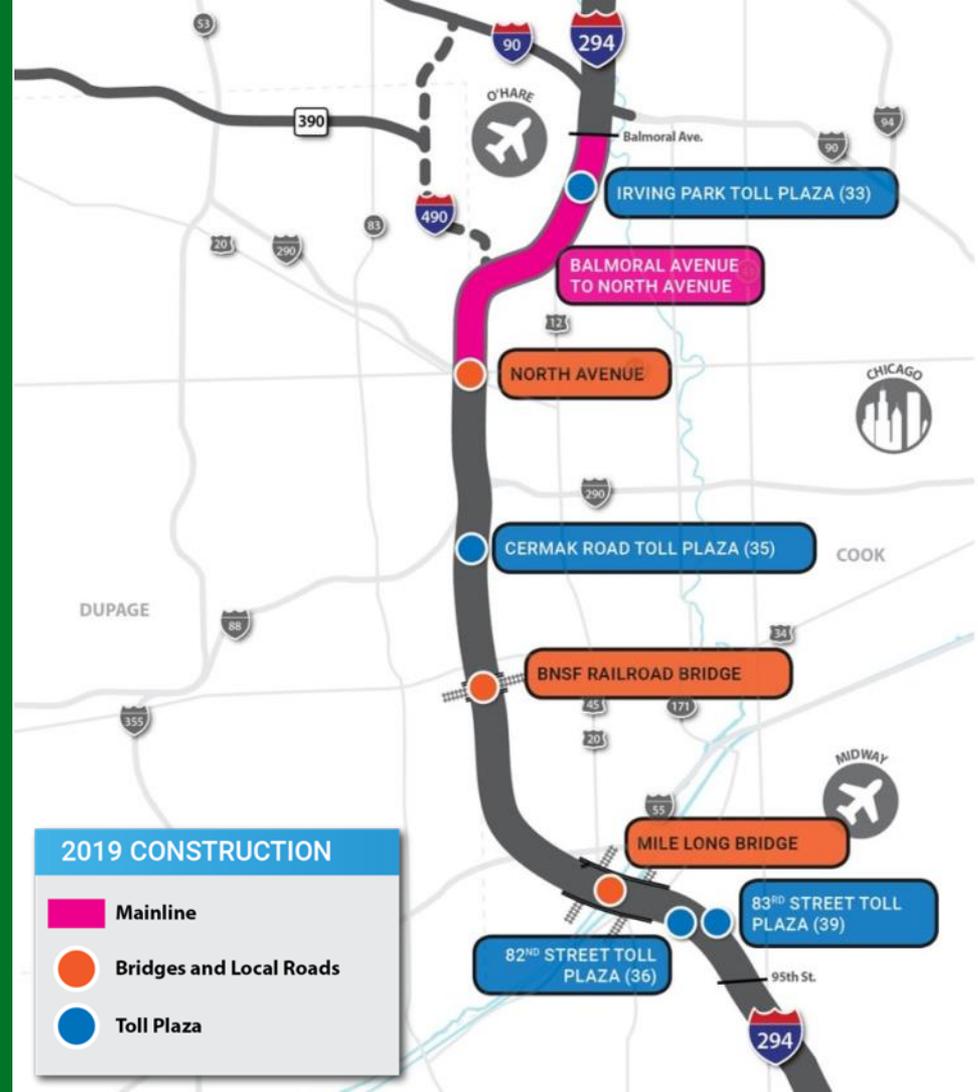
Community concerns

Building removal

Land acquisition

Other

# 2019 Construction



# Fiber Relocation

## PROPOSED CENTRAL TRI-STATE TEMPORARY FIBER

- Temporary Fiber on Wood Poles/Aerial
- █ Temporary Fiber in Conduit
- █ Advanced Tree Removal Route
- Future I-490 Tollway



# Aesthetics



# Drainage and Stormwater



# Corridor Planning Council Recommendations

## To address environmental impacts and support sustainability within the Central Tri-State Corridor:

- Consider collaborative ways to address existing drainage needs through **innovative green infrastructure techniques**.
- Provide **stormwater storage** for all new impervious areas and strive to achieve local goals.
- Improvements should not further degrade water quality of adjacent waterways and a **corridor water quality volume goal** should be evaluated to capture the first flush of rain events.
- Strive to **maintain the natural environment** within the corridor by avoiding and minimizing impacts and improving connectivity between natural resources and communities.

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# Central Tri-State Tollway Design Standards

**Maintain drainage boundaries of watersheds**

**Maintain stormwater release rates**

**Ponding of stormwater on properties is not permitted**

**Prevent erosion on Tollway and adjacent properties**

**Drainage systems must be maintainable, without risk of damage to Tollway or adjacent properties**

**Existing field conditions and recorded pavement flooding are to be taken into account**



The Illinois Tollway  
**DRIVING  
THE FUTURE**

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# Remove and Replace Cross Culverts to Reduce Restrictions

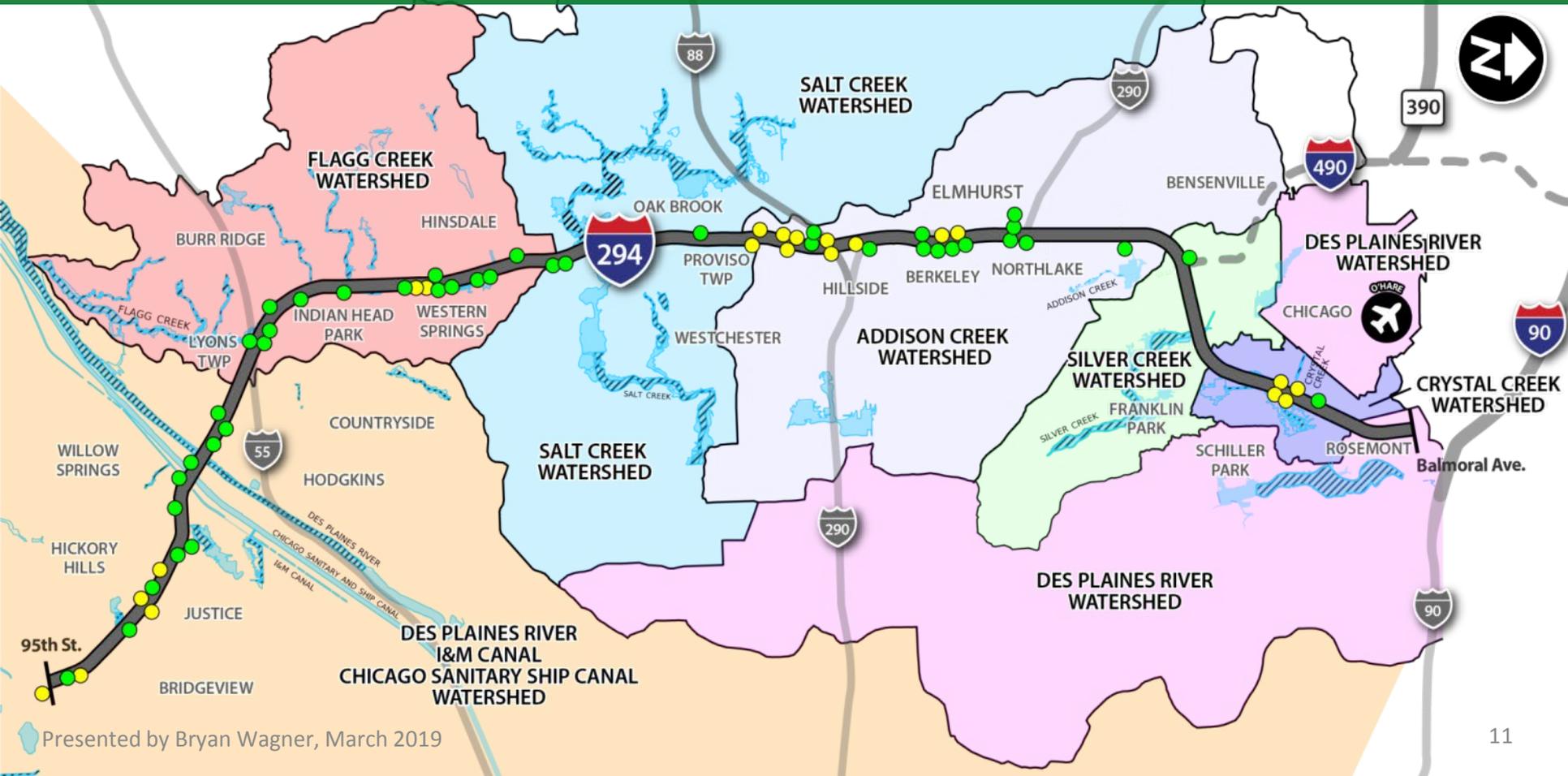


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# Upsize Storm Sewer Conveyance System and Inlet Spacing



# Watersheds



# Water Quality

**Water Quality Volume** is the amount of stormwater runoff from a rainfall event that is captured and treated to remove the majority of stormwater pollutants

- Central Tri-State - 1.00 inch water quality volume (minimum)
  - Accounted for on a sub-watershed basis

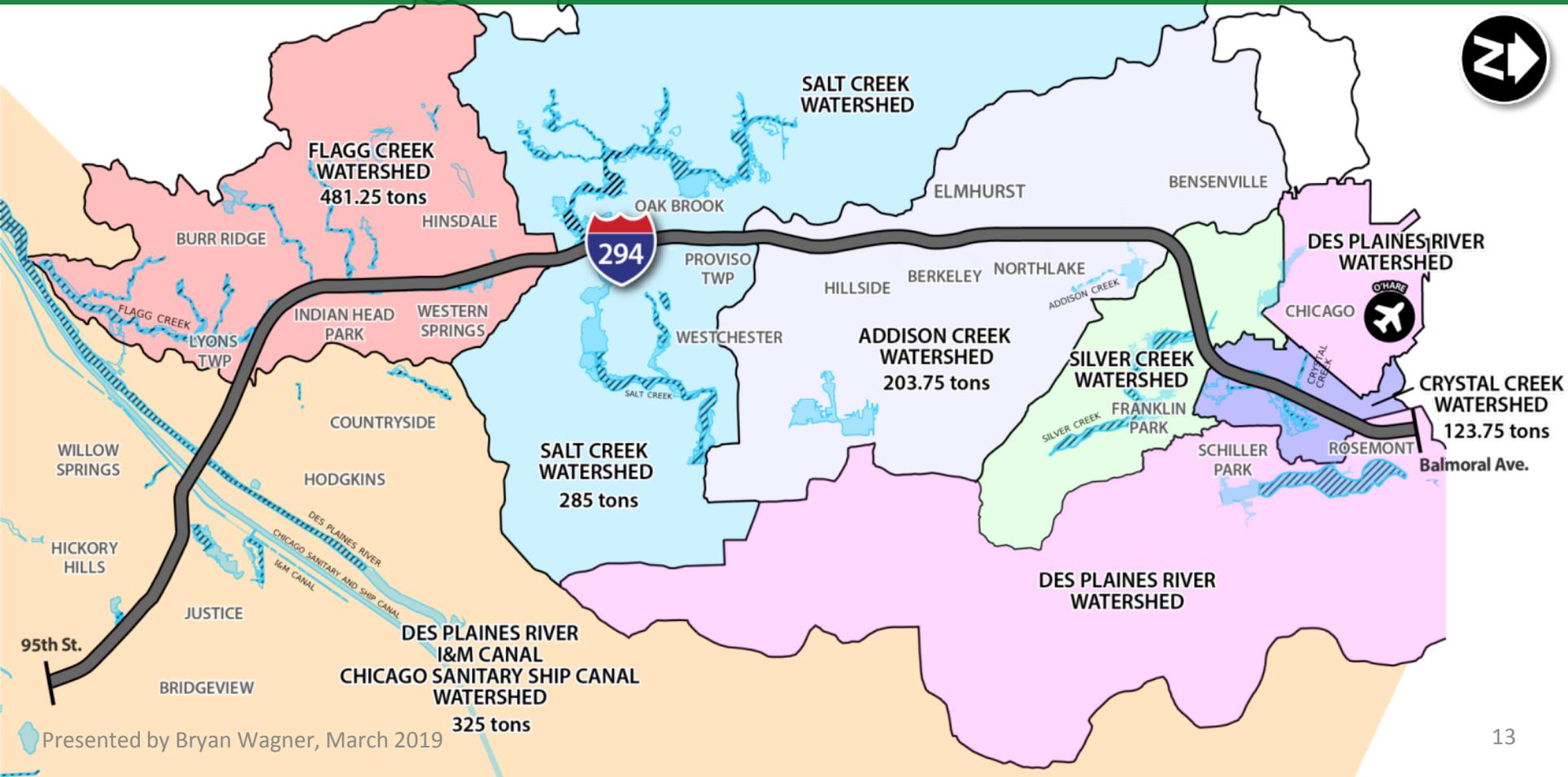
## Means for achieving water quality volume

- Detention basins
  - Basins (dry and wet) and underground box options
- Bioswales



The Illinois Tollway  
**DRIVING  
THE FUTURE**

# Chloride Reductions and Offsets



# Goals and Benefits

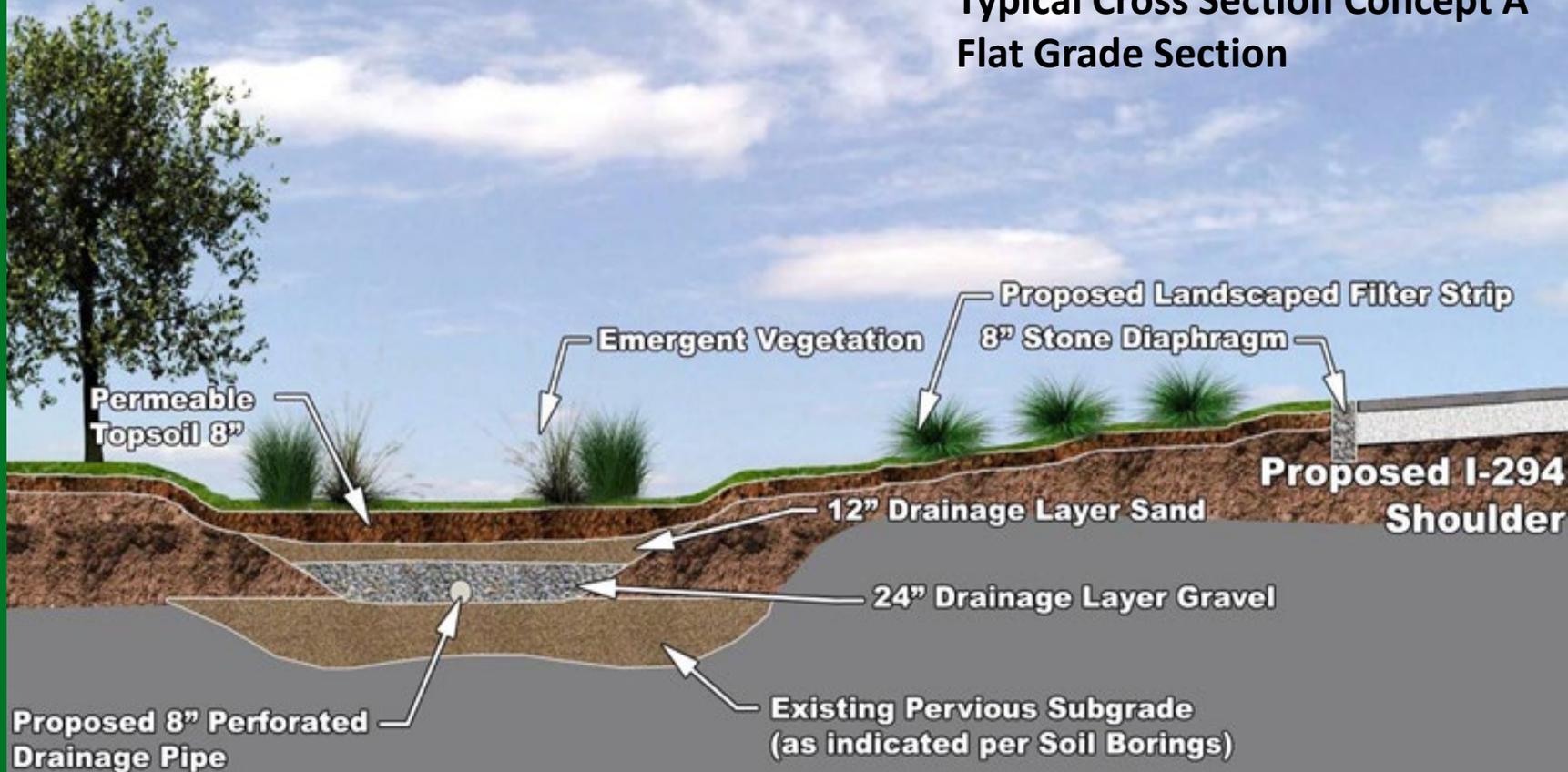
**Partnerships providing regional detention to alleviate flooding**

**Compensatory storage for Central Tri-State related fill-in the flood plain**

**Providing regional detention for watersheds**

# Bioswale Concept

## Proposed Bioswale Typical Cross Section Concept A Flat Grade Section



The Illinois Tollway  
**DRIVING  
THE FUTURE**

# Outreach and Communications

## MATERIALS

Memo

Drainage Report

Map



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# I-294 Travel Demand Analysis

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## Study Update and Findings

December 20, 2018

## Why this Study was Undertaken

Central Tri-State Corridor Committee recommended that I-294 access points be re-evaluated prior to capital program commencement. Given the corridor's location straddling the county line, DuPage County and Cook County formed a partnership to perform a pre-engineering analysis of constraints and opportunities in the corridor.

## Goals

- ✓ Assess community perspectives regarding potential access changes
- ✓ Identify possible accessibility and mobility benefits
- ✓ Report the results of travel model

# Study Area

- » North Limit: IL-38 (Roosevelt Road)
- » South Limit: I-55
- » East Limit: Approx. Mannheim Road
- » West Limit: West of US 83

The Central Tri-State Study Area is approximately 7 miles in length and 6 miles wide.

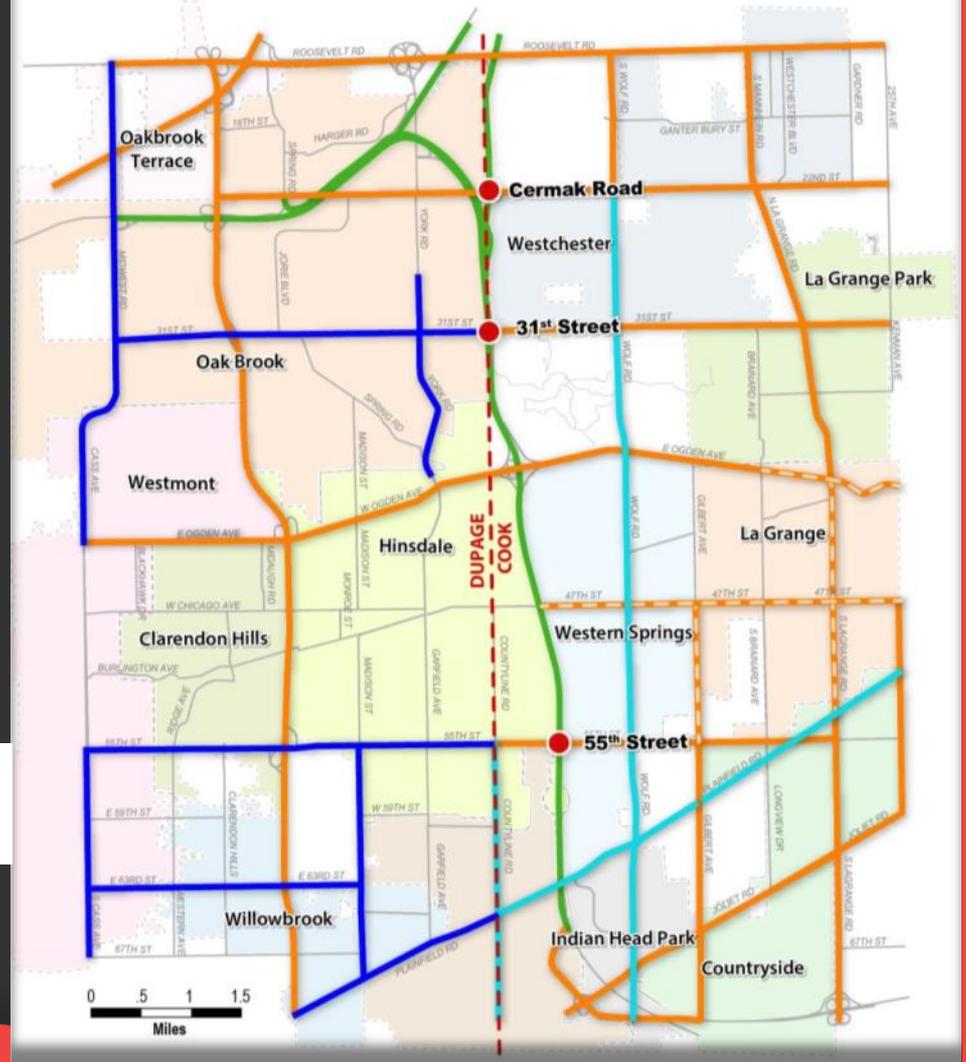


# Nine Concepts Heard from Stakeholders

ID	CONCEPT
1	I-294 at North Avenue - NB exit ramp
2	IL-38 at I-294 - Geometric changes / modernization
3	SB I-294 access from York Road fishhook ramp
4	Spur ramp at I-88/I-294 SB Swift Windsor option
5	Reconfigure 22nd Street access to I-294
6	31st St at I-294 - some access
7	55th Street - I-294 SB Off and NB On Ramps
8	Plainfield Rd - I-294 SB On and NB Off Ramps
9	Wolf Road/71 <sup>st</sup> I-294 Connector - Burr Ridge

# ROADWAY JURISDICTIONS WITH SCENARIO LOCATIONS

- Tollway
- IDOT
- DuPage County
- Scenario Locations:
- Cook County
- - - IDOT - MBO
- Local
- 55th Street (Scenario A)
- 31st Street (Scenario B)
- Cermak Road (Scenario C)



# CONCEPT 3

## SB I-294 Access from York Road Fishhook Ramp (CORE)

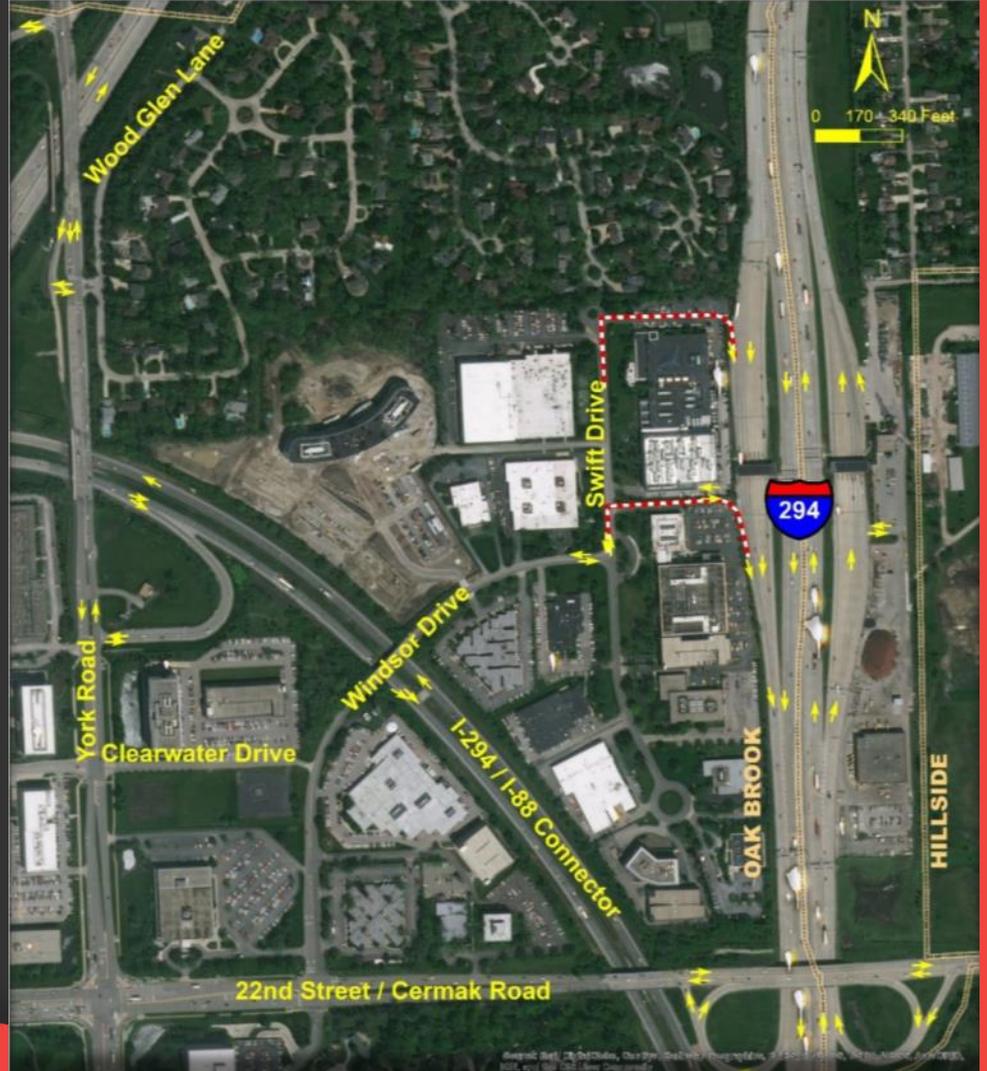
- » **Description:** Add SB I-294 access from York Road at fishhook ramp north of Clearwater at existing SB ramp terminal. Windsor Drive bridge project is moving forward.
- » **Potential Benefits:** Relief on York and Cermak, increased access to I-294.
- » **Issues:** Internal circulation and local congestion



# CONCEPT 4

## Spur Ramp at I-88/I-294 *SB Swift/Windsor Option*

- » **Description:** Add SB I-294 access from Swift Drive. Presumes toll plaza re-design for open road tolling and possible use of ROW, service lanes.
- » **Potential Benefits:** Relief on York and Cermak, increased access to/from I-88/I-294.
- » **Issues:** Tight space (parking and future design of Cermak Plaza).





# CONCEPT 6

## 31st Street at I-294

### *Full Interchange Option*

- » **Description:** Modeled full interchange at 31st Street
- » **Potential Benefits:**
  - Relief to congested US 34/Ogden
  - Relief to congested 22<sup>nd</sup> Street
- » **Issues:** Potential ROW impacts



# CONCEPT 7

## 55th Street - I-294

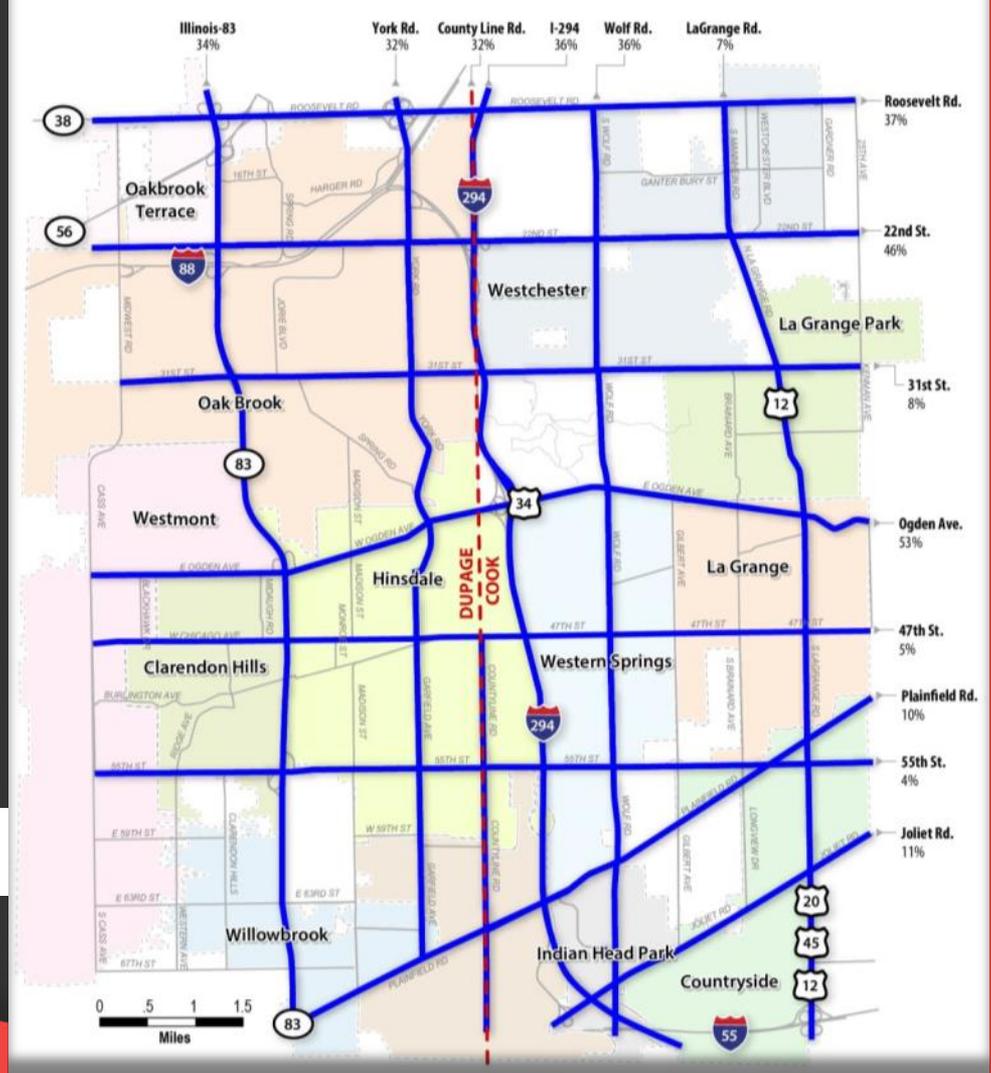
### *SB Off and NB On Ramps Option*

- » **Description:** Half interchange SB off and NB on at 55th Street
- » **Potential Benefits:**
  - Relief on Ogden and the system
  - Relief of Out of Direction travel south of US 34
  - Relief of congestion on local north-south arterials
- » **Issues:**
  - Uncertain access to Oasis south of 55<sup>th</sup>
  - Limited right of way



# AVERAGE ROADWAY GROWTH 2015-2040

*(Without Scenarios)*



# 2040 Traffic Change

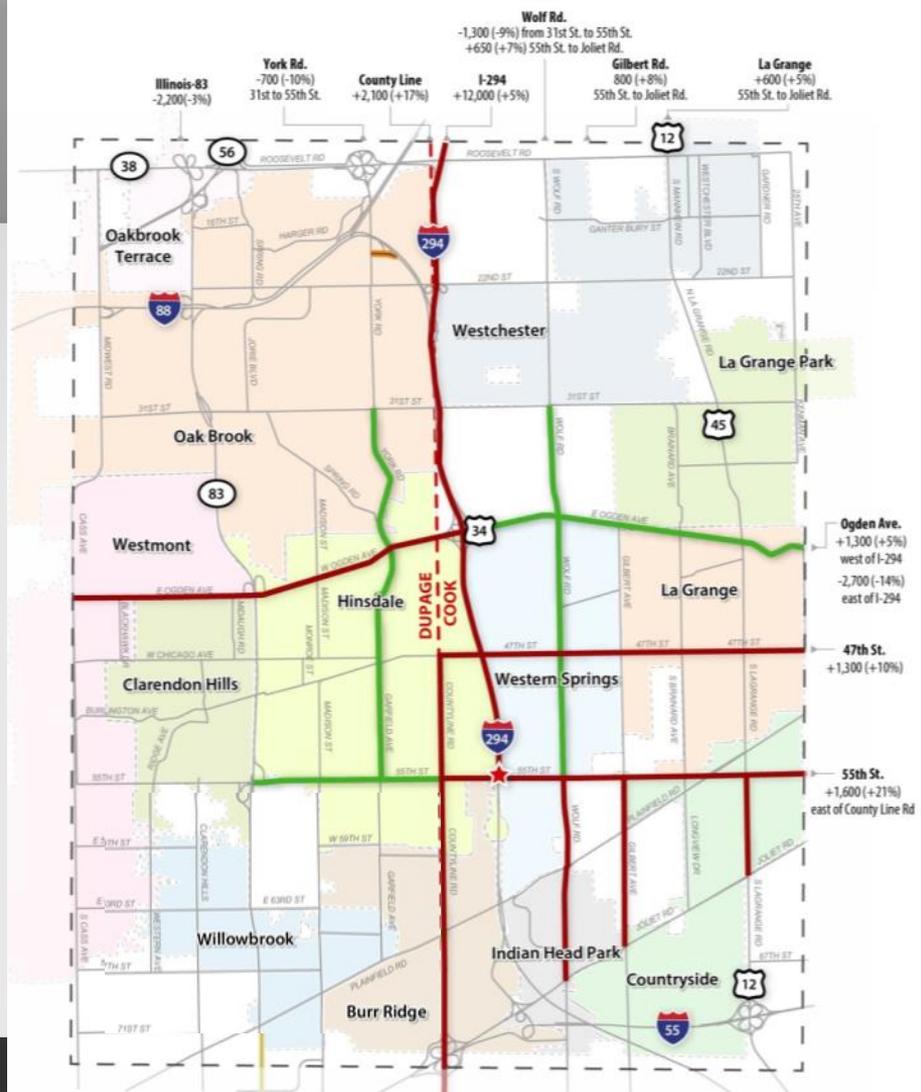
## SCENARIO A

SCENARIO B

SCENARIO C

## 55<sup>th</sup> STREET NEW PARTIAL INTERCHANGE (To/From North)

### 2040 Average Daily Traffic (ADT) Change



# 2040 Traffic Change

SCENARIO A

**SCENARIO B**

SCENARIO C

## 31<sup>st</sup> STREET NEW FULL INTERCHANGE

## 2040 Average Daily Traffic (ADT) Change

- ★ 31st St. New Full Interchange
  - County
  - - - I-294 Study Area
  - Proposed Fishhook Ramp
- ADT Change from the Base:**
- ADT Decrease of 5% or more
  - Minimal Change
  - ADT Increase of 5% or more



# 2040 Traffic Change

SCENARIO A

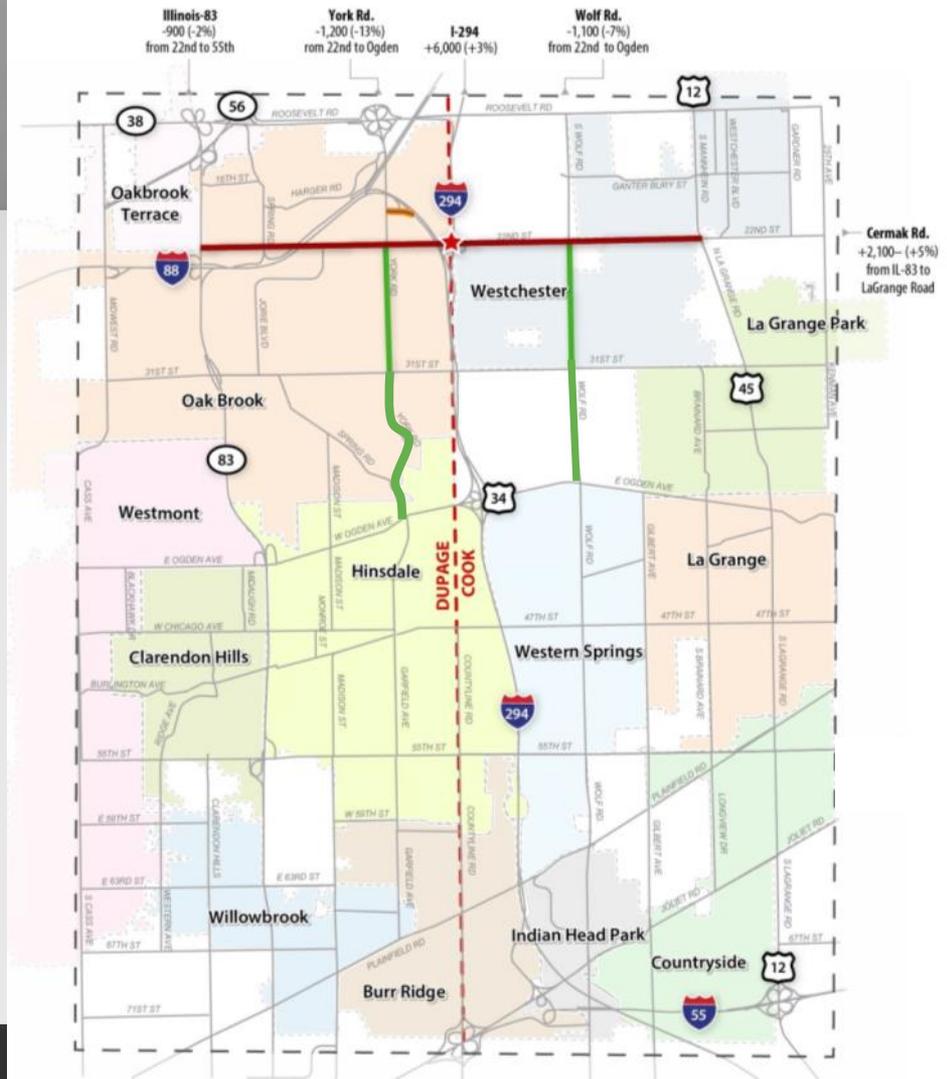
SCENARIO C

**SCENARIO C**

## CERMAK ROAD MODIFIED TO FULL INTERCHANGE

### 2040 Average Daily Traffic (ADT) Change

- ★ Cermak Rd. Full Interchange    - - - County    - - - I-294 Study Area    - - - Proposed Fishhook Ramp
- ADT Change from the Base:**
- ADT Decrease of 5% or more    — Minimal Change    — ADT Increase of 5% or more

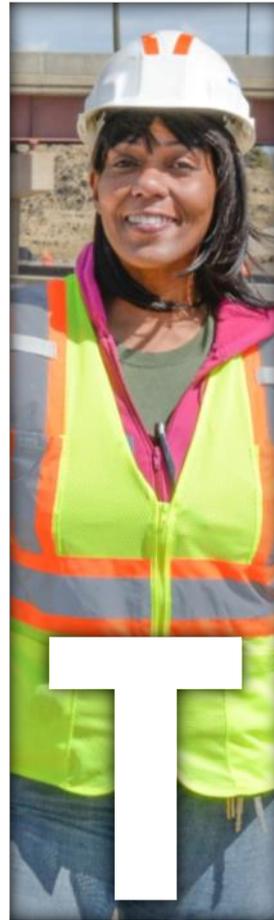
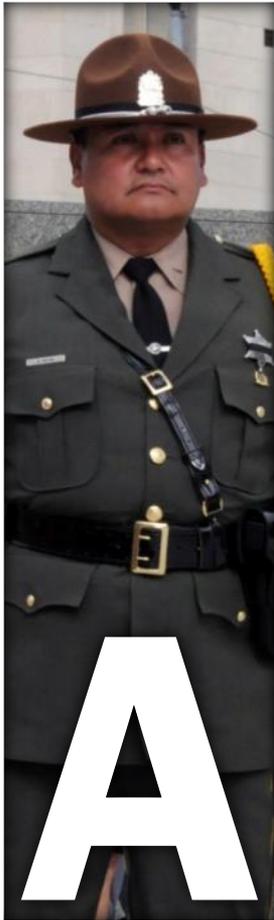


# Summary Matrix

	55th Street at I-294	31st Street at I-294	22nd St./Cermak Rd. at I-294
<b>2 Mile Statistics</b>	Population = 38,073 Employment = 23,369 Businesses = 2,327	Population = 21,111 Employment = 45,490 Businesses = 3,156	Population = 24,918 Employment = 50,545 Businesses = 2,929
<b>Existing Condition</b>	No Interchange	No Interchange	Partial Interchange To/From North
<b>Configuration Tested</b>	Partial Interchange - To/From North	Full Interchange	Full Interchange (Add To/From South)
<b>Jurisdictions</b>	Tollway IDOT	Tollway IDOT, DuPage County	Tollway IDOT
<b>Constraints</b>	Right-of-Way No signal hardware Bridge width	Viaduct and Noise Walls all Quadrants Bridge Width Merge with I-88 to north	Toll Plaza - Mainline merge I-88/I-294 Ramp merge
<b>Ramp Volumes</b>	2040 Projected Ramp Volumes 3000 - 3500 ADT	2040 Projected Ramp Volumes 8000 - 8500 ADT	2040 Projected Ramp Volumes 7900 - 8500 ADT
<b>Traffic Results</b>	<ul style="list-style-type: none"> <li>↓ Wolf Rd. Traffic Decrease (-9%)</li> <li>↓ York Rd./Garfield Ave. Traffic Decrease (-10%)</li> <li>↓ US 34/Ogden Ave. Cook Co Traffic Decrease (-14%)</li> <li>↑ 47th Street Traffic Increase (+10%)</li> <li>↑ County Line Road Traffic Increase (+17%)</li> </ul>	<ul style="list-style-type: none"> <li>↓ Cermak Road Traffic Decrease (-5%)</li> <li>↓ York Rd Traffic Decrease (-21%)</li> <li>↓ US 34/Ogden Ave Cook Co Traffic Decrease (-6%)</li> <li>↑ 31st Street Traffic Increase (+11%)</li> <li>↑ Wolf Road Traffic Increase (+12%)</li> </ul>	<ul style="list-style-type: none"> <li>↓ Wolf Road Traffic Decrease (-7%)</li> <li>↓ York Rd/Garfield Ave Traffic Decrease (-13%)</li> <li>↓ 22nd Street congestion relief west Oak Brook</li> <li>↑ 22nd Street /Cermak Traffic Increase (+5%)</li> </ul>
<b>Considerations</b>	Mixed benefit on US 34/Ogden and at interchange Attracts longer trips to I-294	Mixed benefit to north-south arterials Benefits 22nd Street and US 34 parallel routes	Benefits Oak Brook and Westchester north-south arterials Slight traffic benefit to IL 38/Roosevelt Rd Attracts limited traffic to I-294

# OTHER THINGS HEARD

- ▲ Ogden Avenue - noted by most as a problem location
- ▲ Flex Lanes as truck lanes (incentives for trucks to stay on Tollway)
- ▲ Oak Brook commercial revitalization plan cites numerous slip ramps as aid in mitigating Cermak Road congestion and out-of-direction traffic.
- ▲ Potential positive impacts of longer acceleration lanes.
- ▲ Concerns about I-294/I-55 interchange performance.
- ▲ Early coordination of construction staging/detours requested.

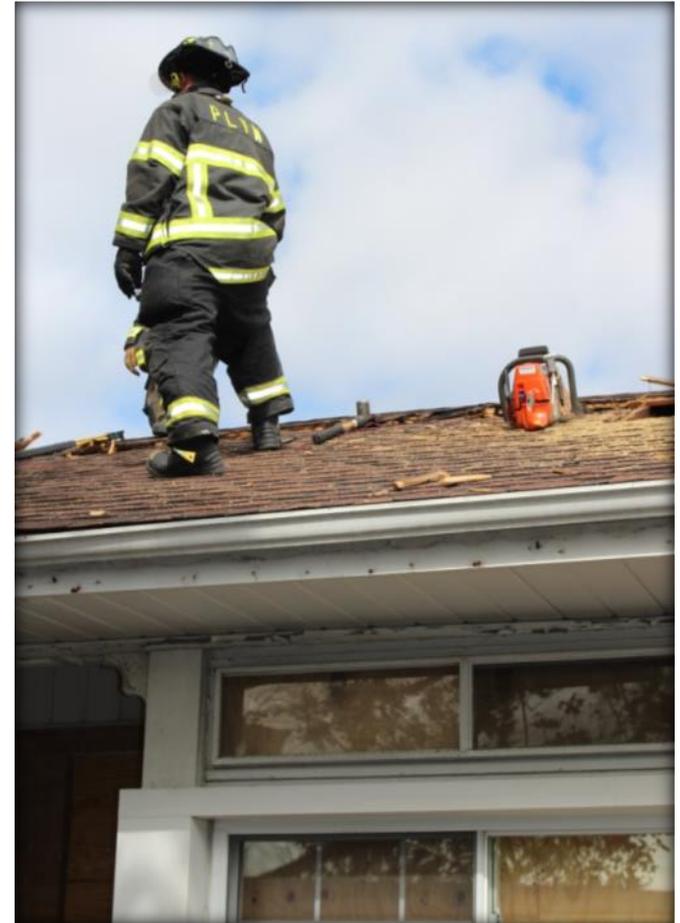


# Building Removal

Tollway procedures to secure and maintain properties

Coordinating with local municipalities

Opportunities for emergency responder training at Tollway-acquired properties



**We Want to Hear  
From You**

**FUTURE TOPICS**



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**THANK YOU**

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